

# 1A KINGSTON COMPLETE STREETS: SR 104

## FERRY COMMUTER/ OUTLYING COMMERCIAL

### PURPOSE:

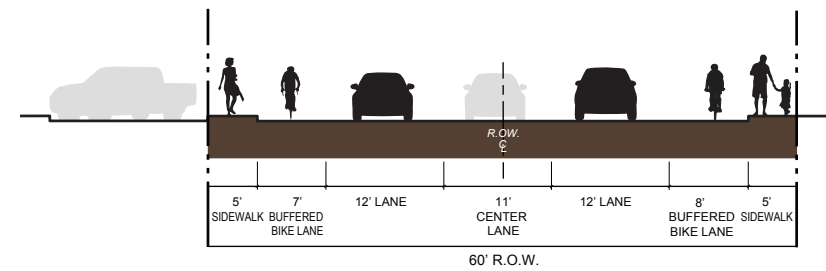
SR 104 is a main commercial corridor and has minimal non-motorized improvements over much of the west half. Street frontage consists of wide, sometimes ill-defined, driveways into commercial establishments, making pedestrian and bicycle travel along the corridor difficult and unsafe. Sidewalk and bike lane facilities will enhance non-motorized travel along this busy commuter and commercial corridor, improving access to businesses and calming traffic which will improve safety.

### DESCRIPTION / FEATURES:

Two lanes with a center turn lane are retained, but sidewalks and buffered bike lanes are added both sides. Buffered bike lanes provide more separation from truck and higher speed traffic. Limiting the number of driveway access points and/or reducing their width is recommended, to improve safety of pedestrians and cyclists crossing traffic. Transit stops are immediately adjacent to the sidewalk and transit/bike lanes may have to share space. Planted medians where possible.

**COSTS: \$2,418,000**

**RANKING: HIGH**



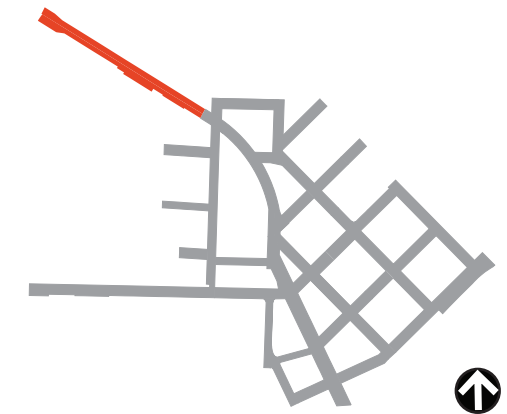
**TYPICAL SECTION**



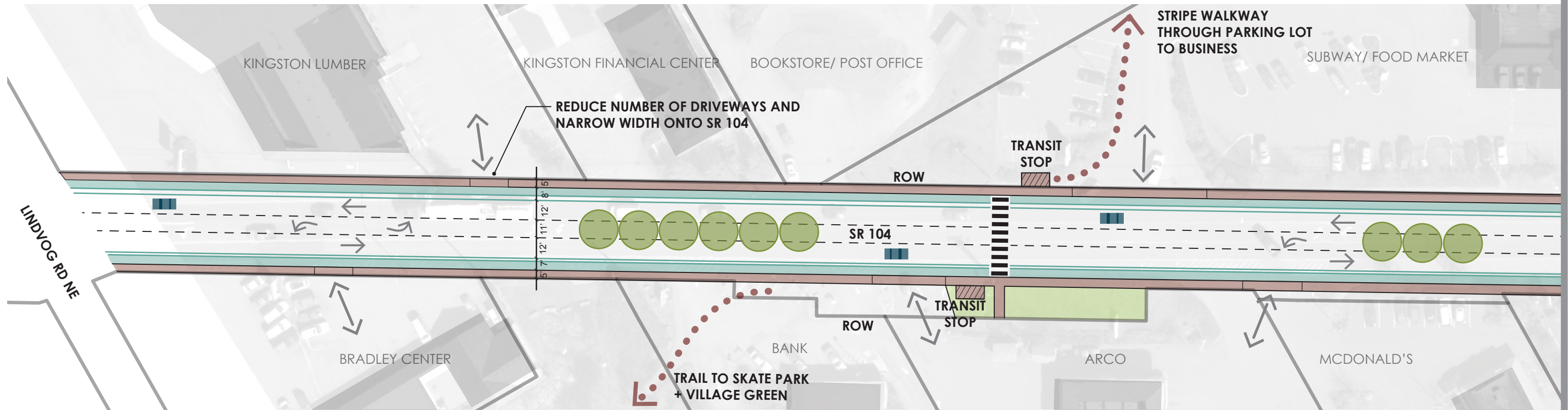
above: consolidated driveways and sidewalk as priority



above: buffered bike lane



**PROJECT KEY**



**PLAN VIEW** 1"=60'

- sharrow
- parcel line
- bioretention swale
- vegetated buffer
- parking
- sidewalk/path
- bike lane
- project limit
- adjacent project number

MATCHLINE A: SEE NEXT SHEET



# 1A KINGSTON COMPLETE STREETS: SR 104

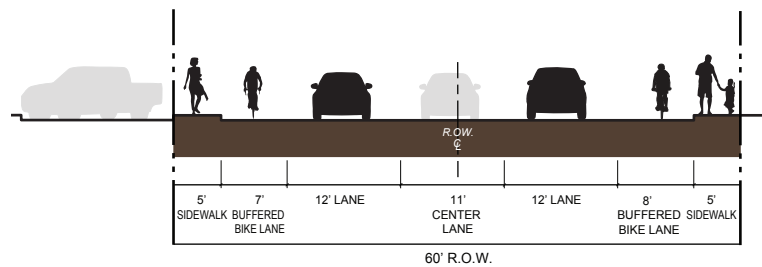
## FERRY COMMUTER/ OUTLYING COMMERCIAL

### DESCRIPTION / FEATURES:

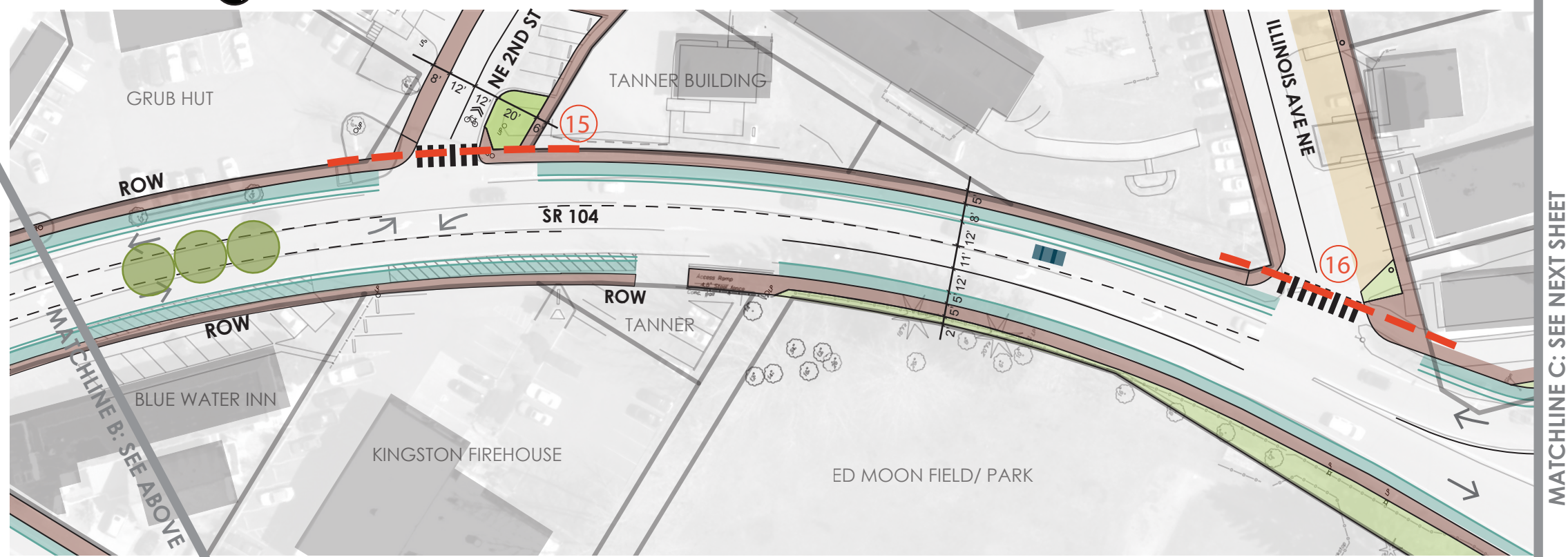
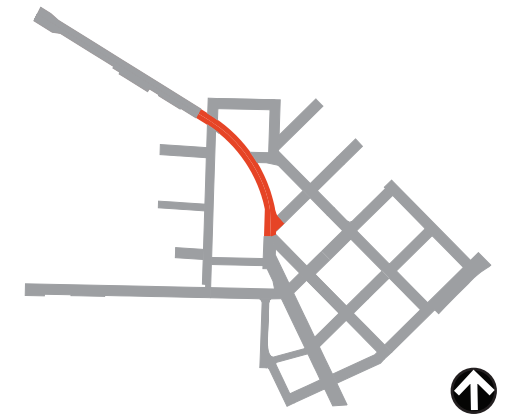
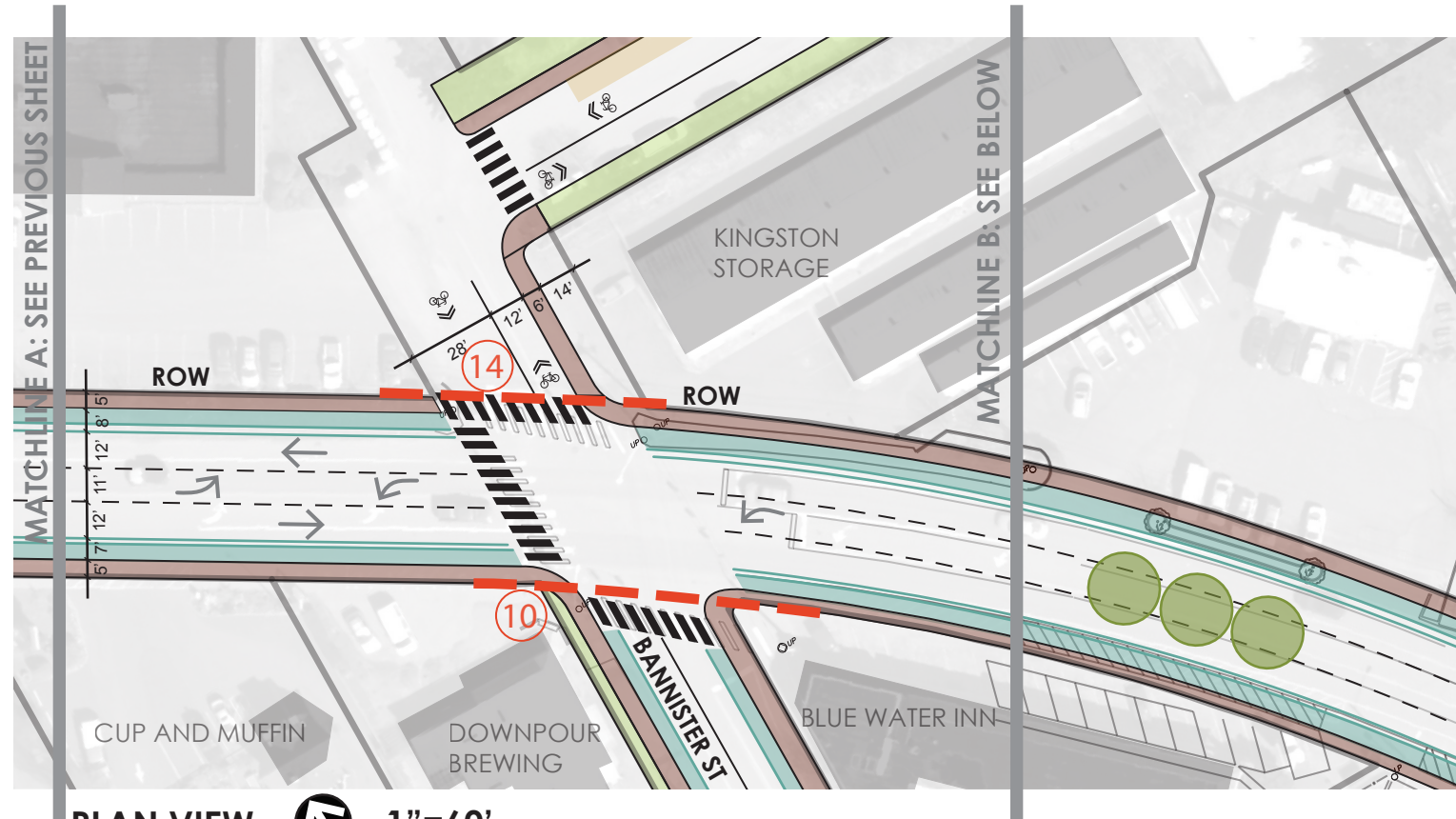
Buffered bike lane transitions to bike lane where ROW width does not allow for the added width of a buffer. Sidewalks are continuous and connect to existing or new sidewalks on side streets. ADA ramps are recommended at all intersections and crosswalks. Parking and access to several businesses will be impacted by frontage improvements, and parking will have to be accommodated elsewhere on these sites. Shared parking opportunities may exist, which would further reduce the number of driveway cuts onto the State Route, improving safety for non-motorized users.



above: ADA curb ramp at street crossing



### TYPICAL SECTION



- sharrow
- parcel line
- bioretention swale
- vegetated buffer
- parking
- sidewalk/path
- bike lane
- project limit
- adjacent project number



# 1A KINGSTON COMPLETE STREETS: SR 104

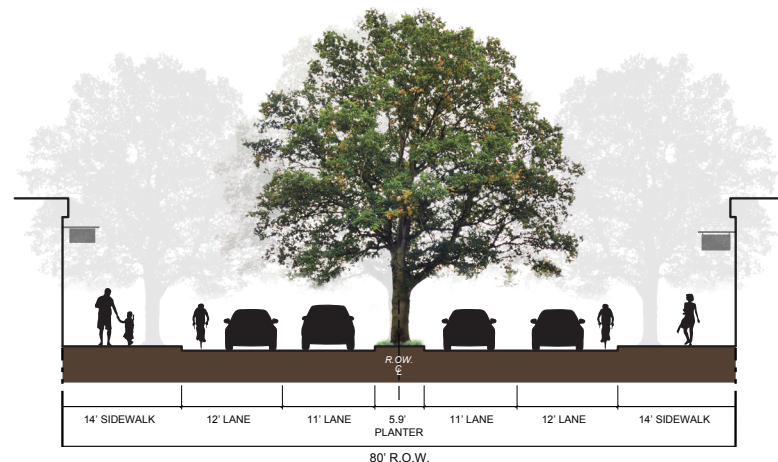
## ACTIVE SHARED USE- MAJOR

### DESCRIPTION / FEATURES:

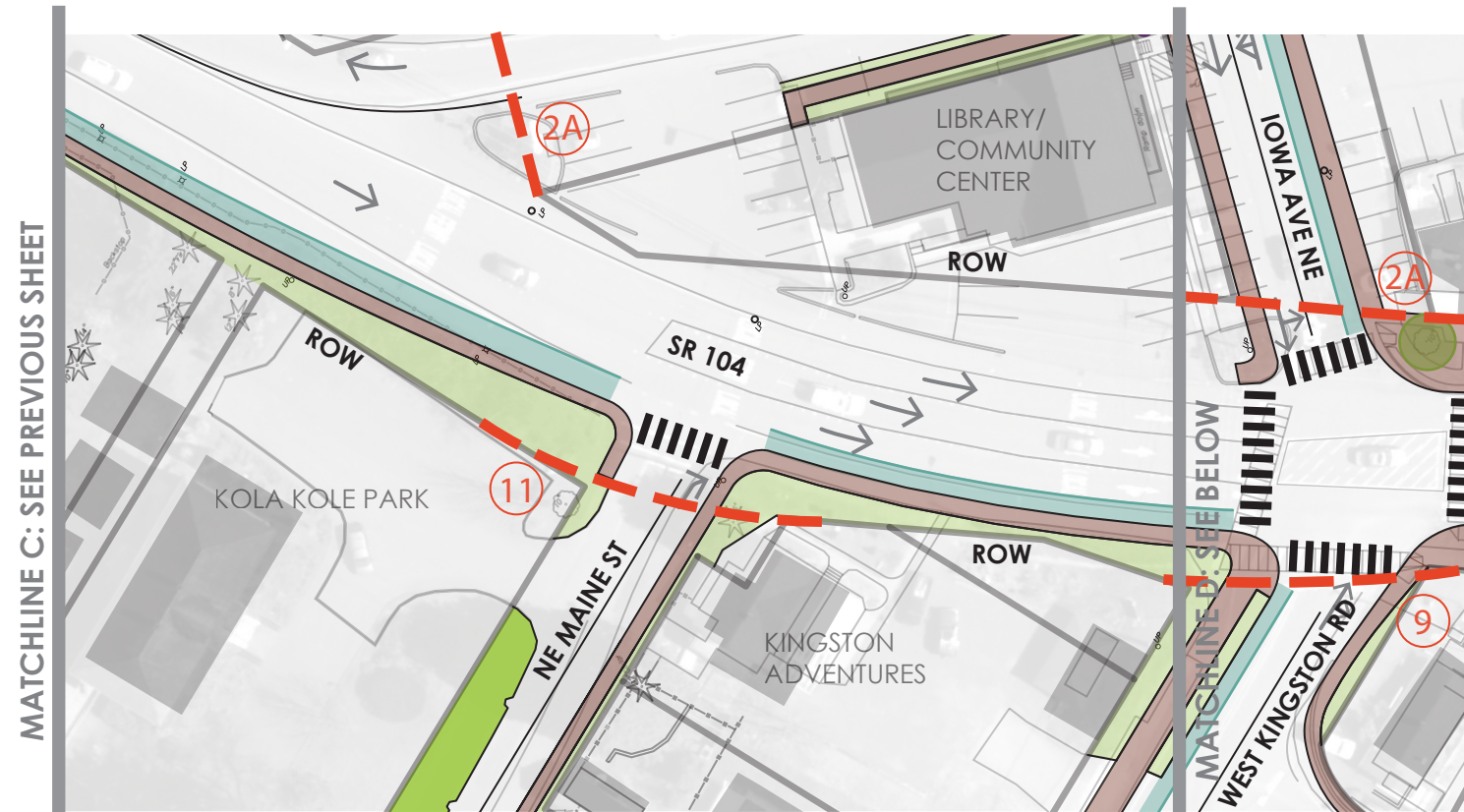
Three lanes transition to 4 lanes, as existing. SE (in) bound bike lane transitions to sharrow in both 'local' lanes through downtown, NW (out) bound bike lane is located on NE 1st Street (see Project 2A). Between Iowa Ave. and Washington Boulevard, sidewalks widen to accommodate sidewalk frontage expansion of business activity and provide for greater volumes of walking traffic. Street tree planting remains on sides and down central median/boulevard. On-street parallel parking is eliminated, and service/delivery vehicles will have to occupy 'local' lane or access from side streets, similar to existing conditions.



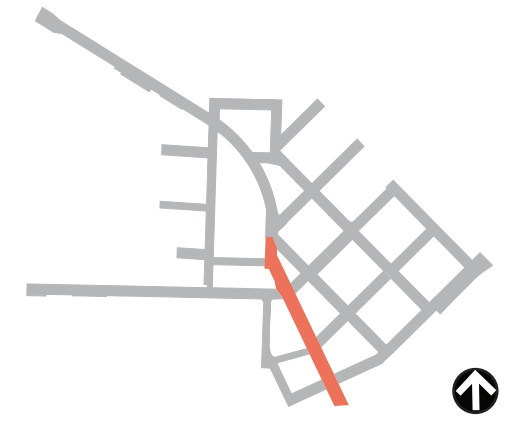
above: wide sidewalks accommodate multiple uses



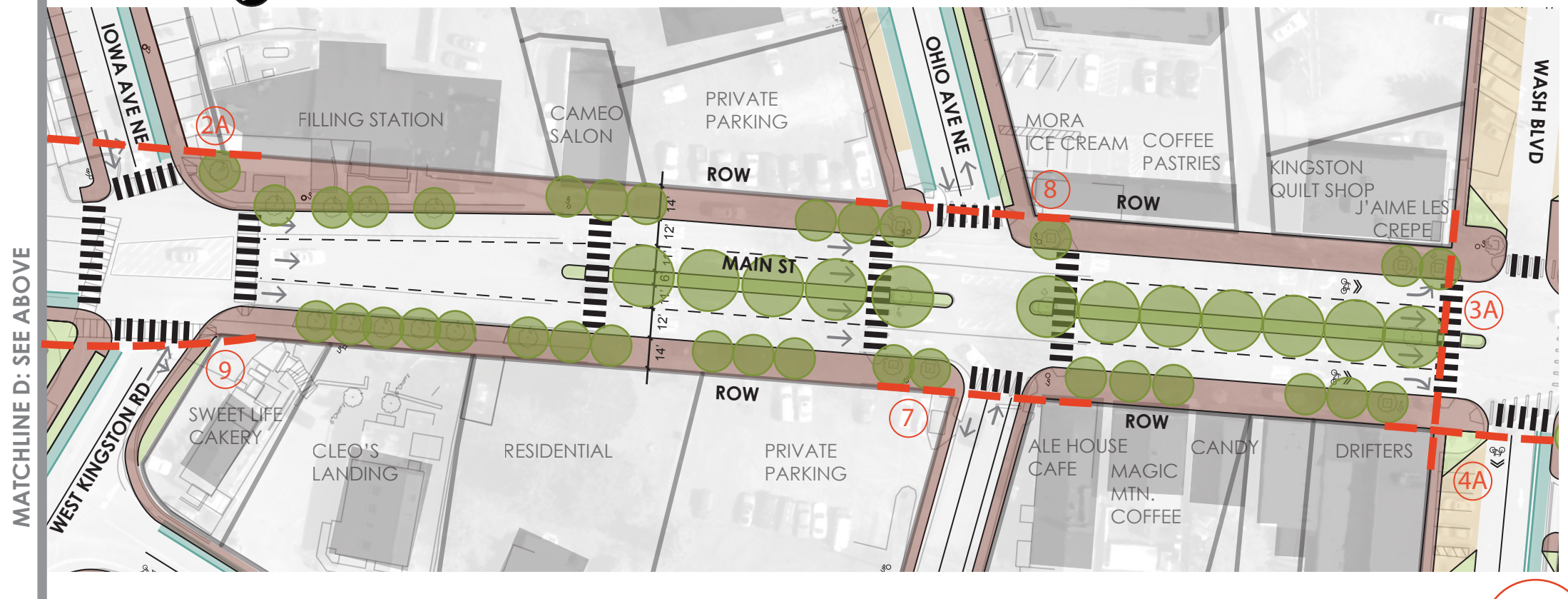
TYPICAL SECTION



PLAN VIEW 1"=60'



PROJECT KEY



PLAN VIEW 1"=60'

- sharrow
- parcel line
- bioretention swale
- vegetated buffer
- parking
- sidewalk/path
- bike lane
- project limit
- adjacent project number



# 1B KINGSTON COMPLETE STREETS: SR 104

## FERRY COMMUTER/ OUTLYING COMMERCIAL

### PURPOSE:

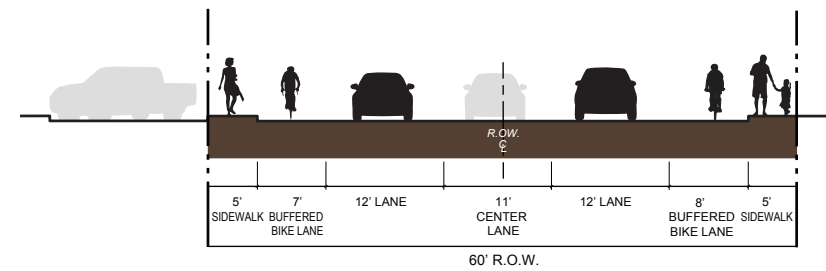
SR 104 is a main commercial corridor and has minimal non-motorized improvements over much of the west half. Street frontage consists of wide, sometimes ill-defined, driveways into commercial establishments, making pedestrian and bicycle travel along the corridor difficult and unsafe. Sidewalk and bike lane facilities will enhance non-motorized travel along this busy commuter and commercial corridor, improving access to businesses and calming traffic which will improve safety.

### DESCRIPTION / FEATURES:

Two lanes with a center turn lane are retained, but sidewalks and buffered bike lanes are added both sides. Buffered bike lanes provide more separation from truck and higher speed traffic. Limiting the number of driveway access points and/or reducing their width is recommended, to improve safety of pedestrians and cyclists crossing traffic. Transit stops are immediately adjacent to the sidewalk and transit/bike lanes may have to share space. Planted medians where possible.

**COSTS: \$4,760,000**

**RANKING: HIGH**



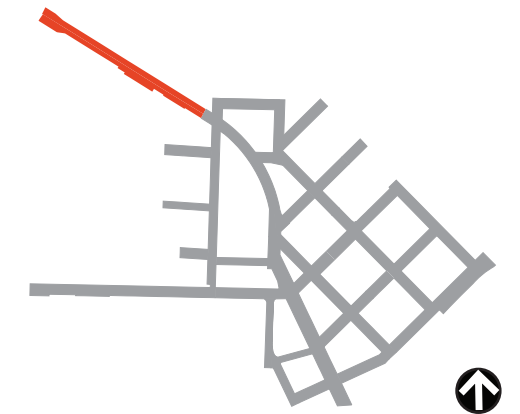
**TYPICAL SECTION**



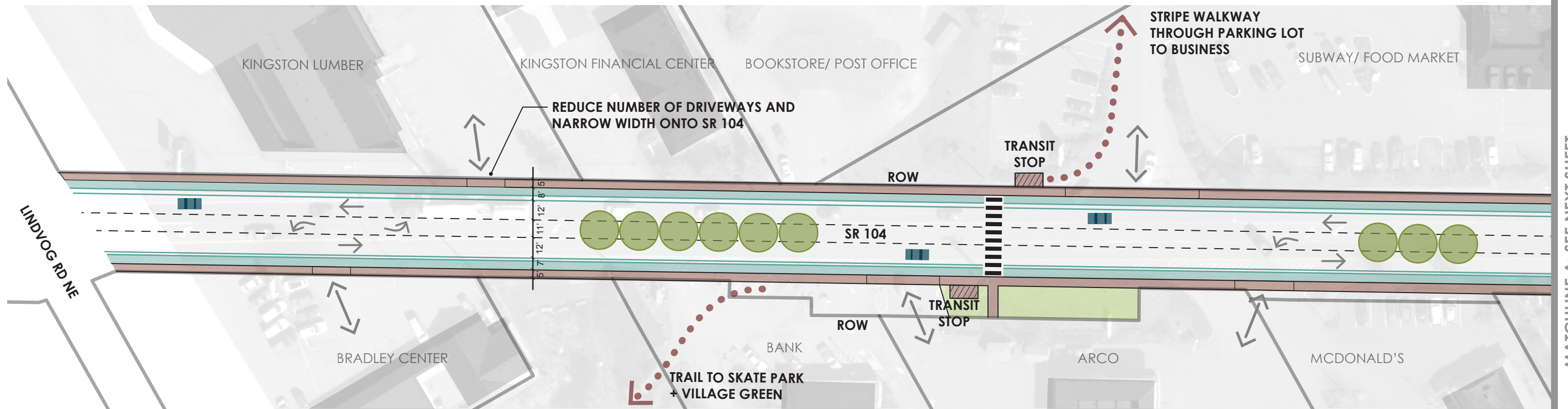
above: consolidated driveways and sidewalk as priority



above: buffered bike lane



**PROJECT KEY**



**PLAN VIEW**



1"=60'

- sharrow
- parcel line
- bioretention swale
- vegetated buffer
- parking
- sidewalk/path
- bike lane
- project limit

16 adjacent project number

1B

MATCHLINE A: SEE NEXT SHEET



# 1B KINGSTON COMPLETE STREETS: SR 104

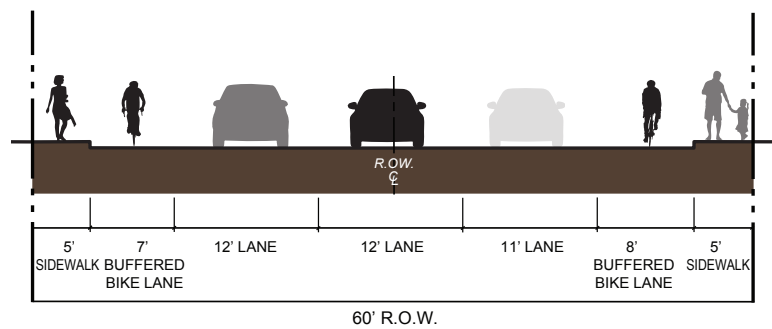
FERRY COMMUTER/ OUTLYING COMMERCIAL

## DESCRIPTION / FEATURES:

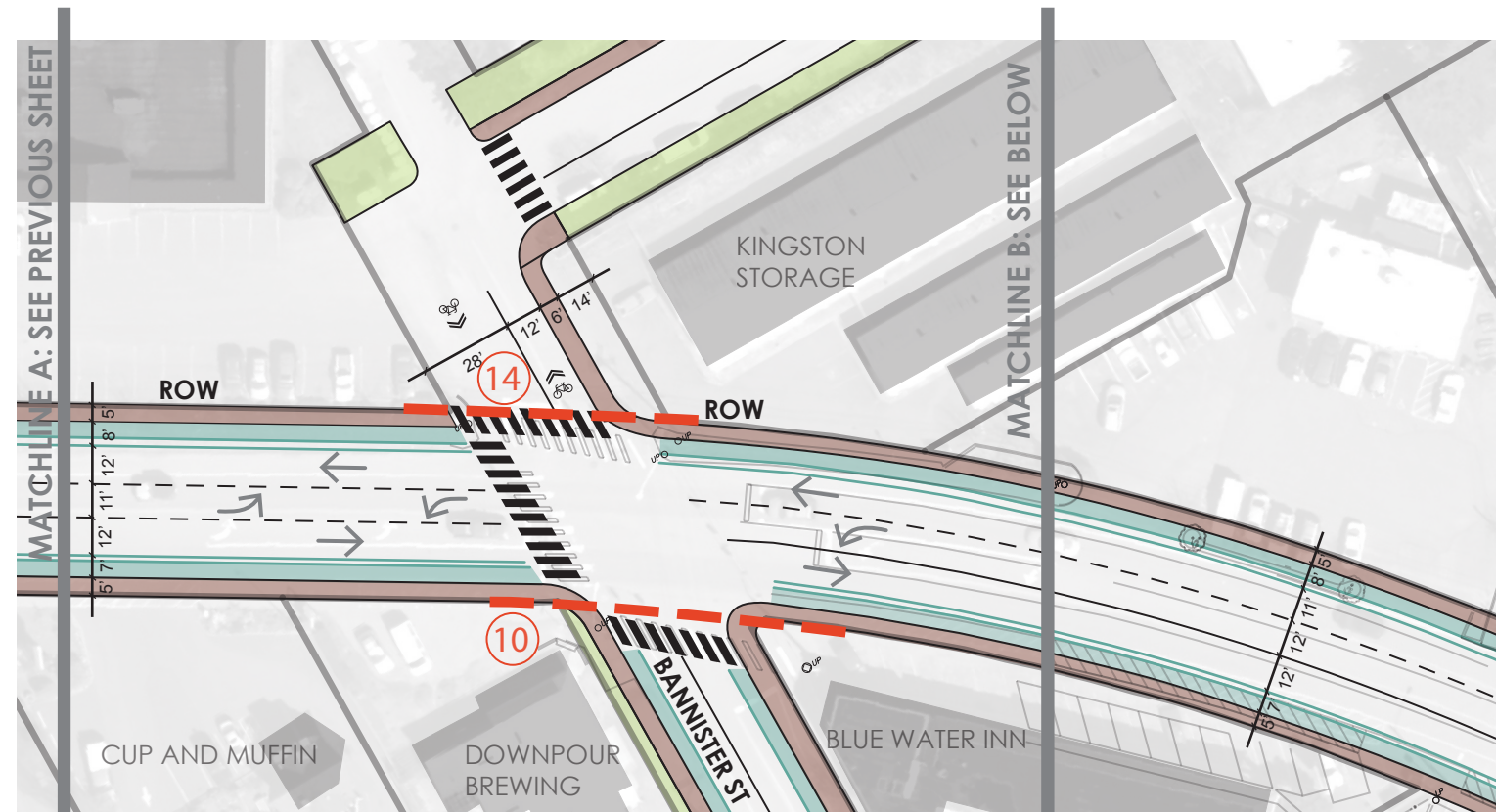
Buffered bike lane transitions to bike lane where ROW width does not allow for the added width of a buffer. Travel lane expands from 1 lane both directions to 2 lanes NW (out) bound at NE 2nd Street to accommodate ingress and egress vehicular traffic to/from ferry. SE (in)bound bike lane follows Main Street, NW (out)bound bike lane follows NE 1st Street. Sidewalks are continuous and connect to existing or new sidewalks on side streets. Parking and access to several businesses will be impacted by frontage improvements, and parking will have to be accommodated elsewhere on these sites. Shared parking opportunities may exist, which would further reduce the number of driveway cuts onto the State Route.



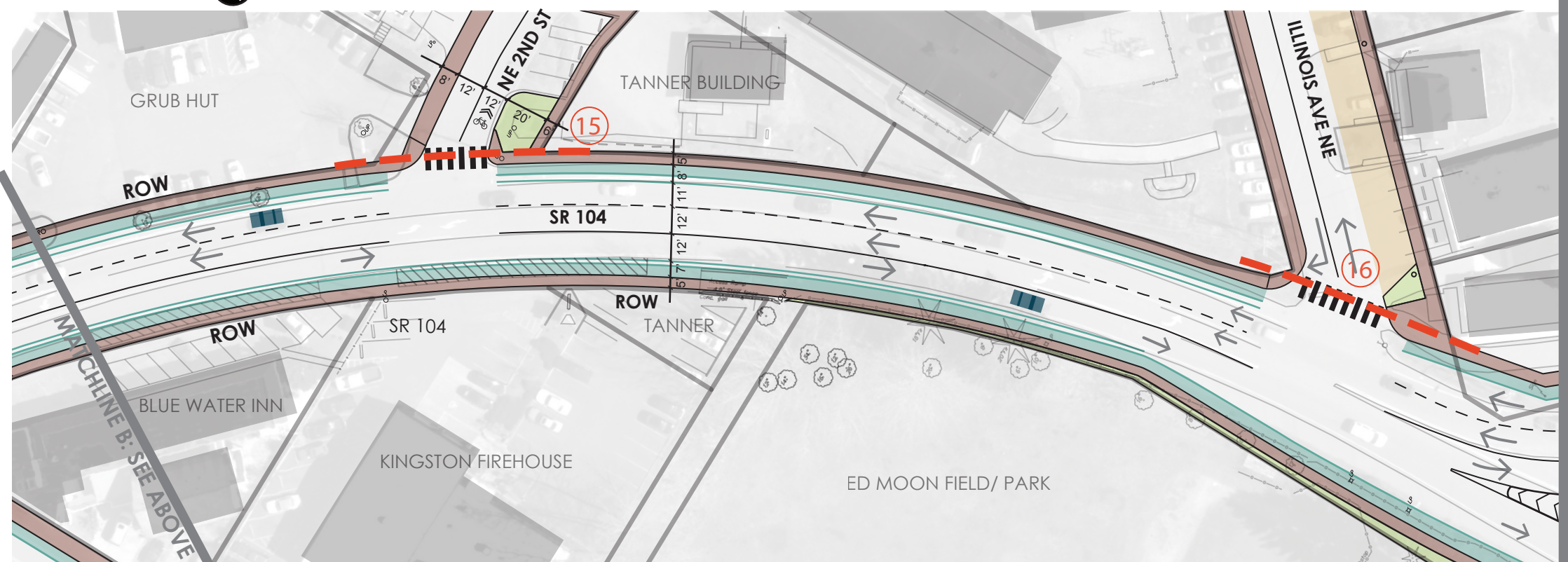
above: buffered bike lane transition to Sharrow



TYPICAL SECTION

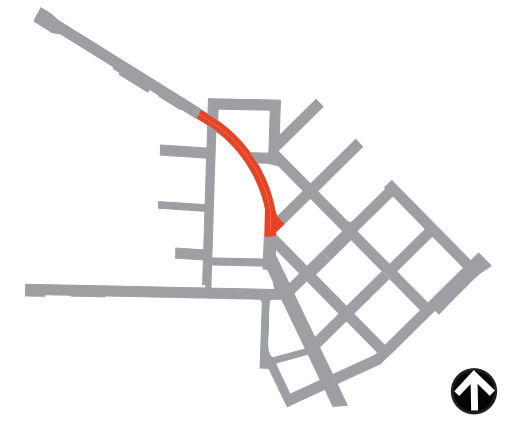


PLAN VIEW 1"=60'



PLAN VIEW 1"=60'

- sharrow
- parking
- adjacent project number
- parcel line
- sidewalk/path
- bioretention swale
- bike lane
- vegetated buffer
- project limit



PROJECT KEY



# 1B KINGSTON COMPLETE STREETS: SR 104

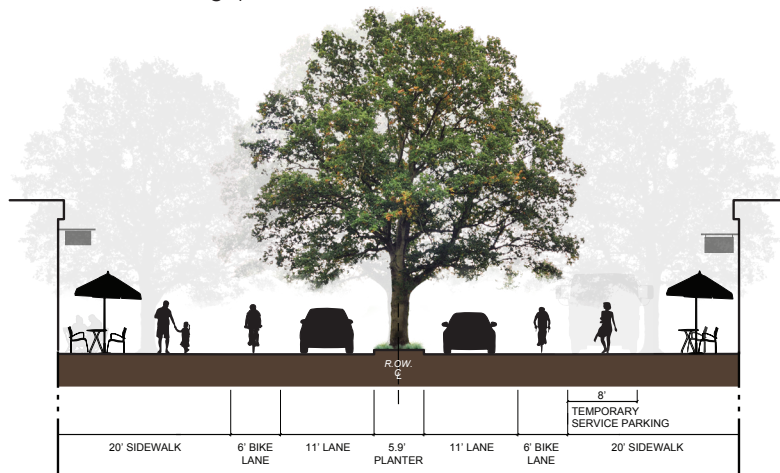
## ACTIVE SHARED USE- MAJOR

### DESCRIPTION / FEATURES:

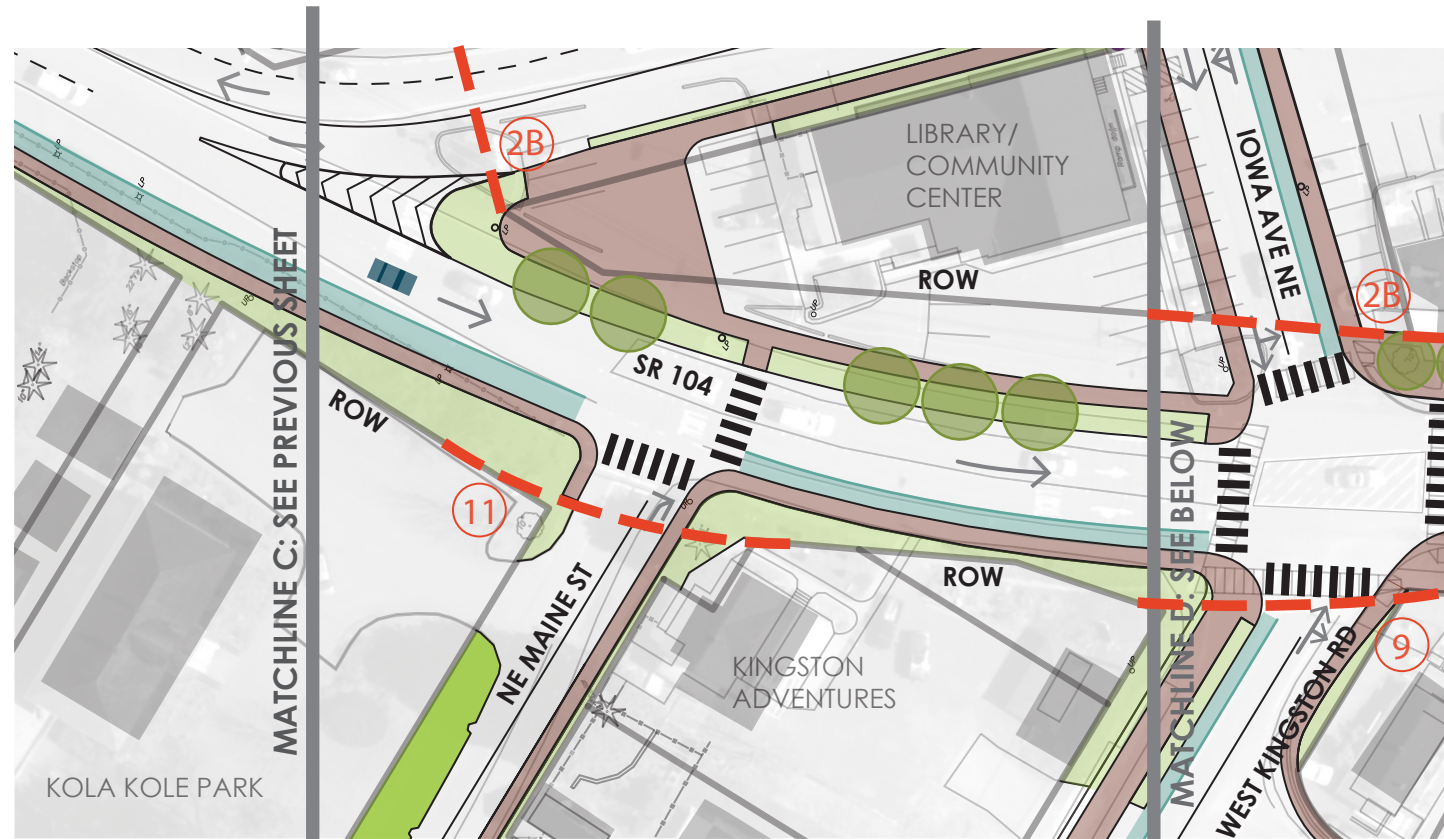
Ferry ingress moves to NE 1st Street, leaving Main Street right lane as a local traffic route. Main Street is now 2 lanes, two-way. SE (in)bound bike lane continues along Main Street to access ferry, NW (out) bound bike lane is also on Main Street to Iowa where it turns east to route bikes away from the 'triangle', to NE 1st Street. Opportunity for public space and/or gateway feature at the north end of the triangle. Between Iowa Avenue and Washington Boulevard, sidewalks widen to accommodate sidewalk frontage expansion of business activity and provide for greater volumes of walking traffic. Potential for convertible street these blocks, with flush or mountable curbs both sides. Street tree planting remains on sides and down central median/boulevard. On-street parallel parking is eliminated, but service/delivery vehicles may have short-term parking in the pedestrian sidewalk space or have access from side streets.



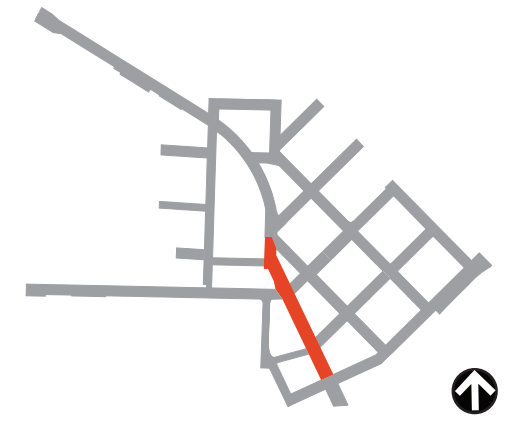
above: Convertible street during farmer's market; the curbsless sidewalk allows for flexible use during special events.



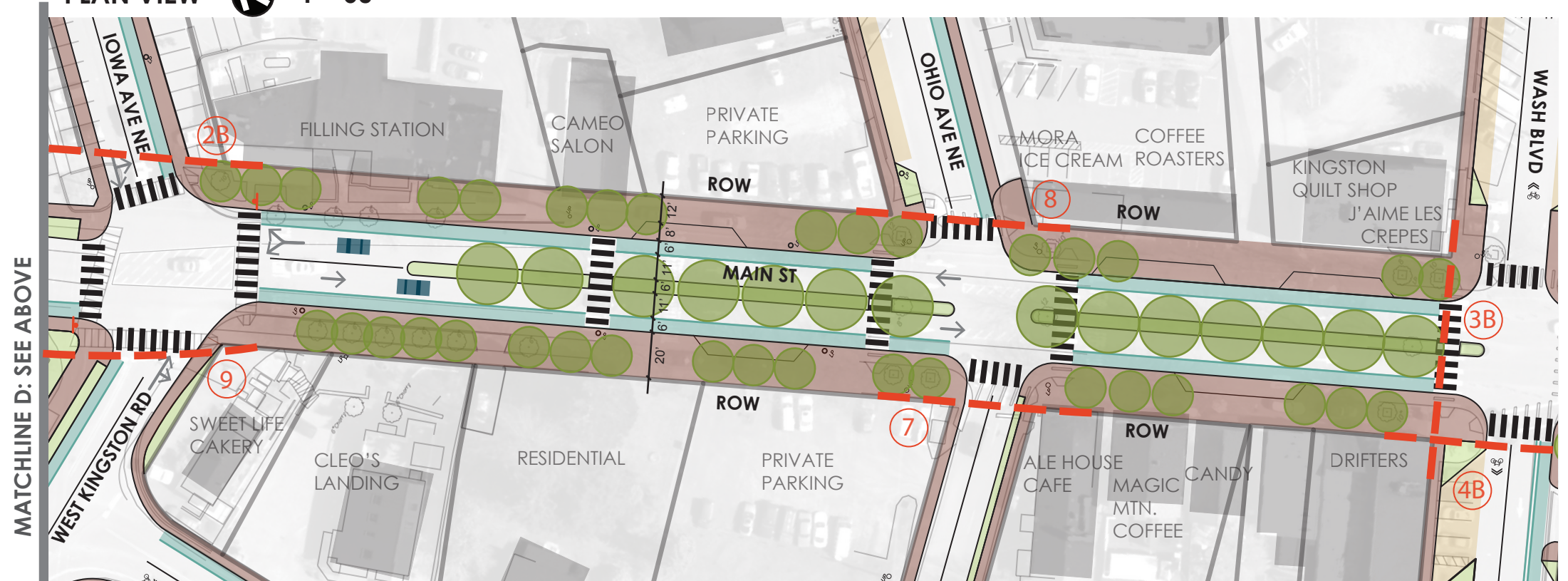
TYPICAL SECTION



PLAN VIEW 1"=60'



PROJECT KEY



PLAN VIEW 1"=60'

- sharrow
- parcel line
- bioretention swale
- vegetated buffer
- parking
- sidewalk/path
- bike lane
- project limit
- adjacent project number



# 2A KINGSTON COMPLETE STREETS: NE 1ST STREET

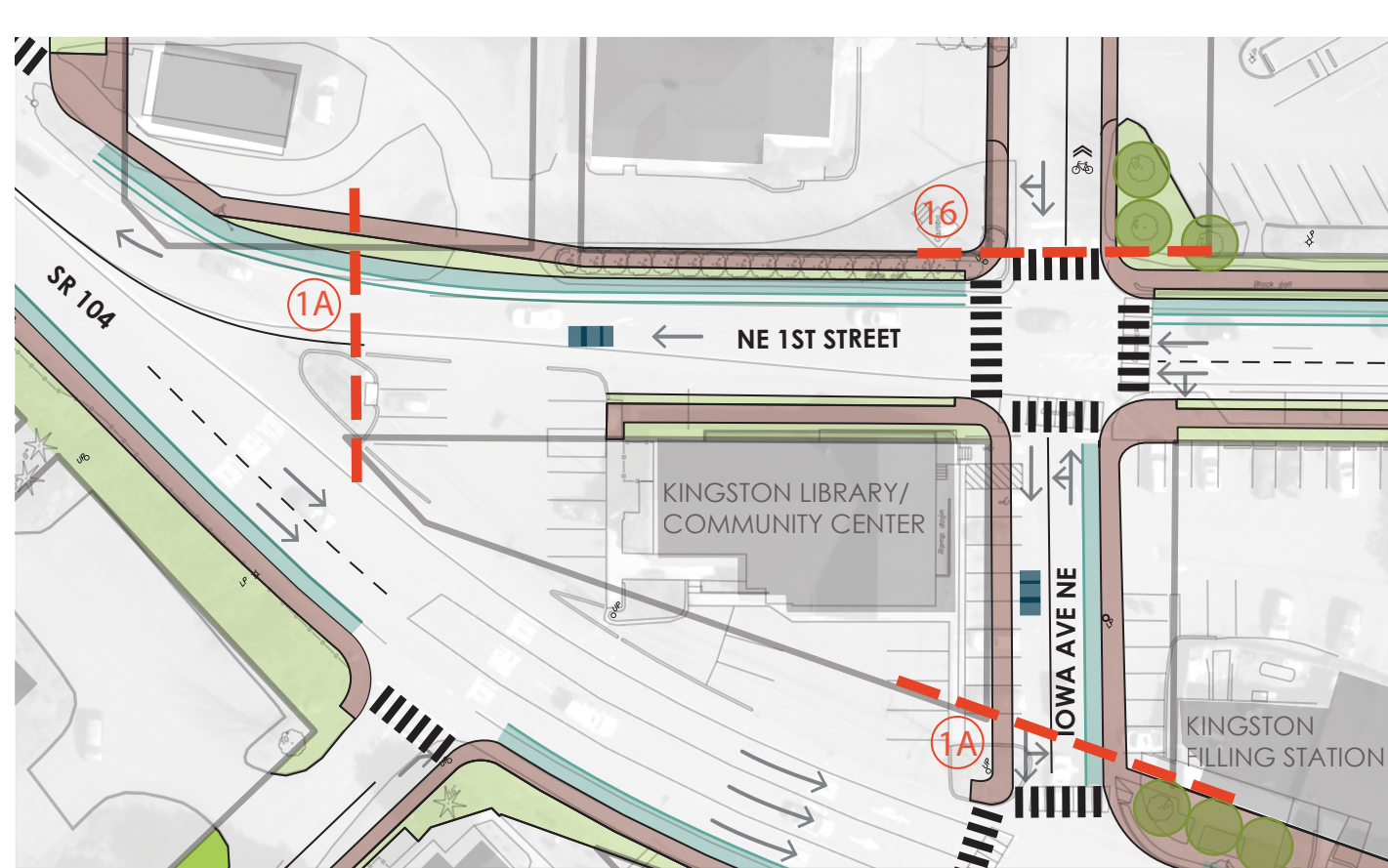
## FERRY COMMUTER

### PURPOSE:

NE 1st Street serves primarily as ferry egress, secondarily as NW outbound access from downtown businesses and residential. Currently no non-motorized facilities exist along this segment, except for an unsurfaced path at the WSDOT parking lot, making pedestrian and bicycle travel along the corridor difficult and unsafe. Sidewalk, ADA ramp, crosswalk, and bike lane facilities will improve safety and enhance non-motorized travel to and from businesses and the ferry.

### DESCRIPTION / FEATURES:

Two lanes NW (out) bound travel lanes are retained, with a merge to one lane occurring between Iowa Avenue and SR 104. Sidewalks with a planting strip are added both sides, and a buffered bike lane provides outbound bike traffic with more separation from truck and higher speed traffic. Crosswalk and ADA ramp improvements are made at both Ohio and Iowa intersections, improving safety for those crossing NE 1st Street. Utility relocation and retaining wall relocation will be required in this corridor to accommodate non-motorized improvements.



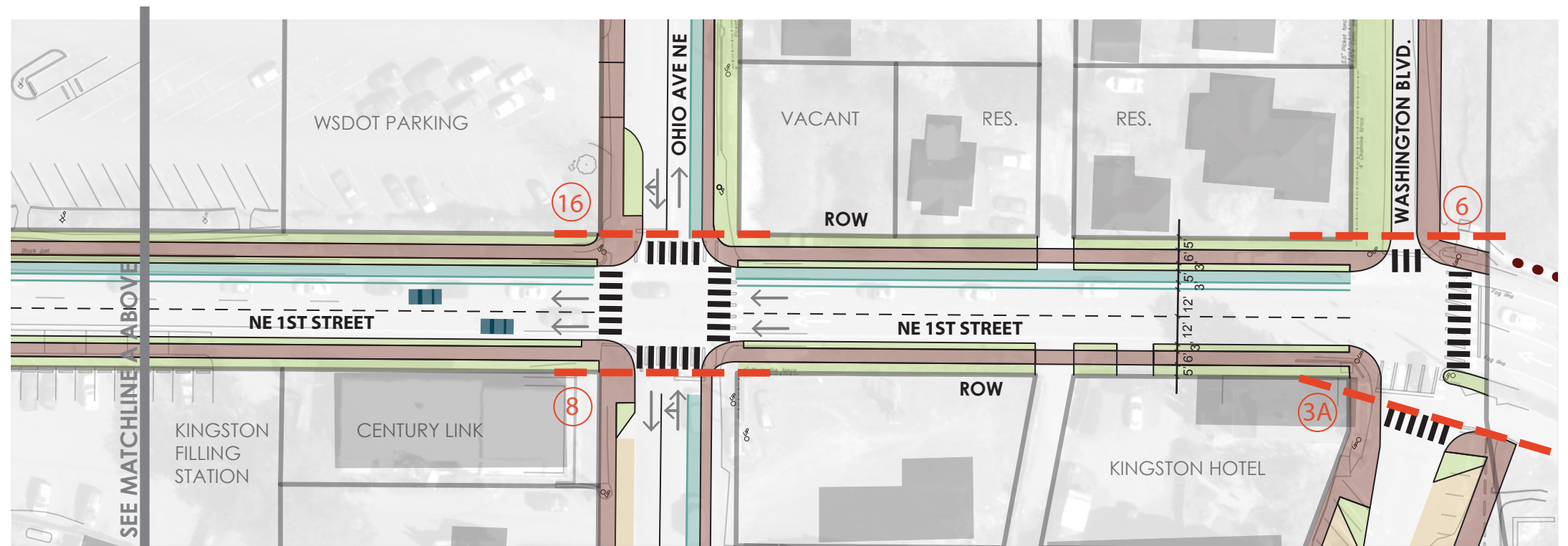
SEE MATCHLINE A BELOW



PROJECT KEY

COSTS: \$1,605,000

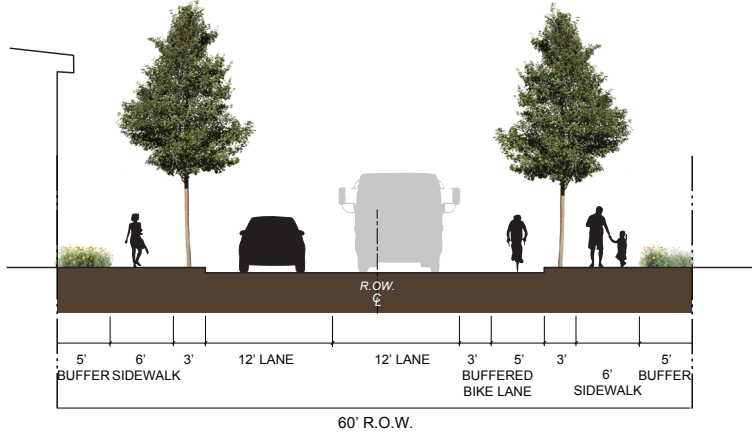
RANKING: HIGH



PLAN VIEW

1"=60'

- sharrow
- parcel line
- bioretention swale
- vegetated buffer
- parking
- sidewalk/path
- bike lane
- project limit
- adjacent project number



TYPICAL SECTION



above: buffered bike lane, planting and sidewalk





# 2B KINGSTON COMPLETE STREETS: NE 1ST STREET

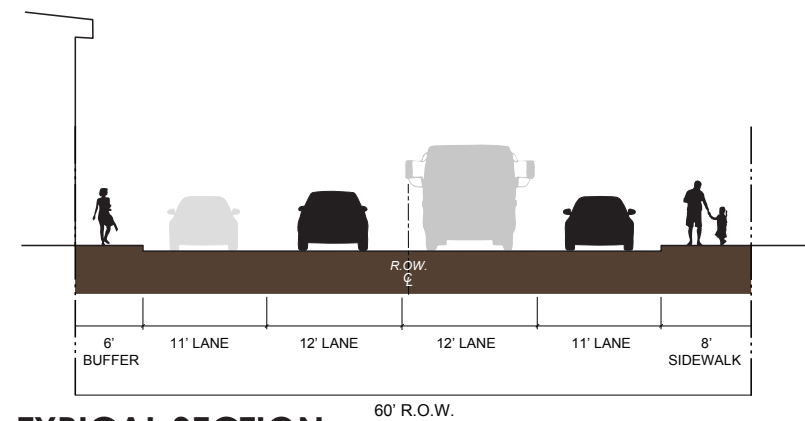
## FERRY COMMUTER

### PURPOSE:

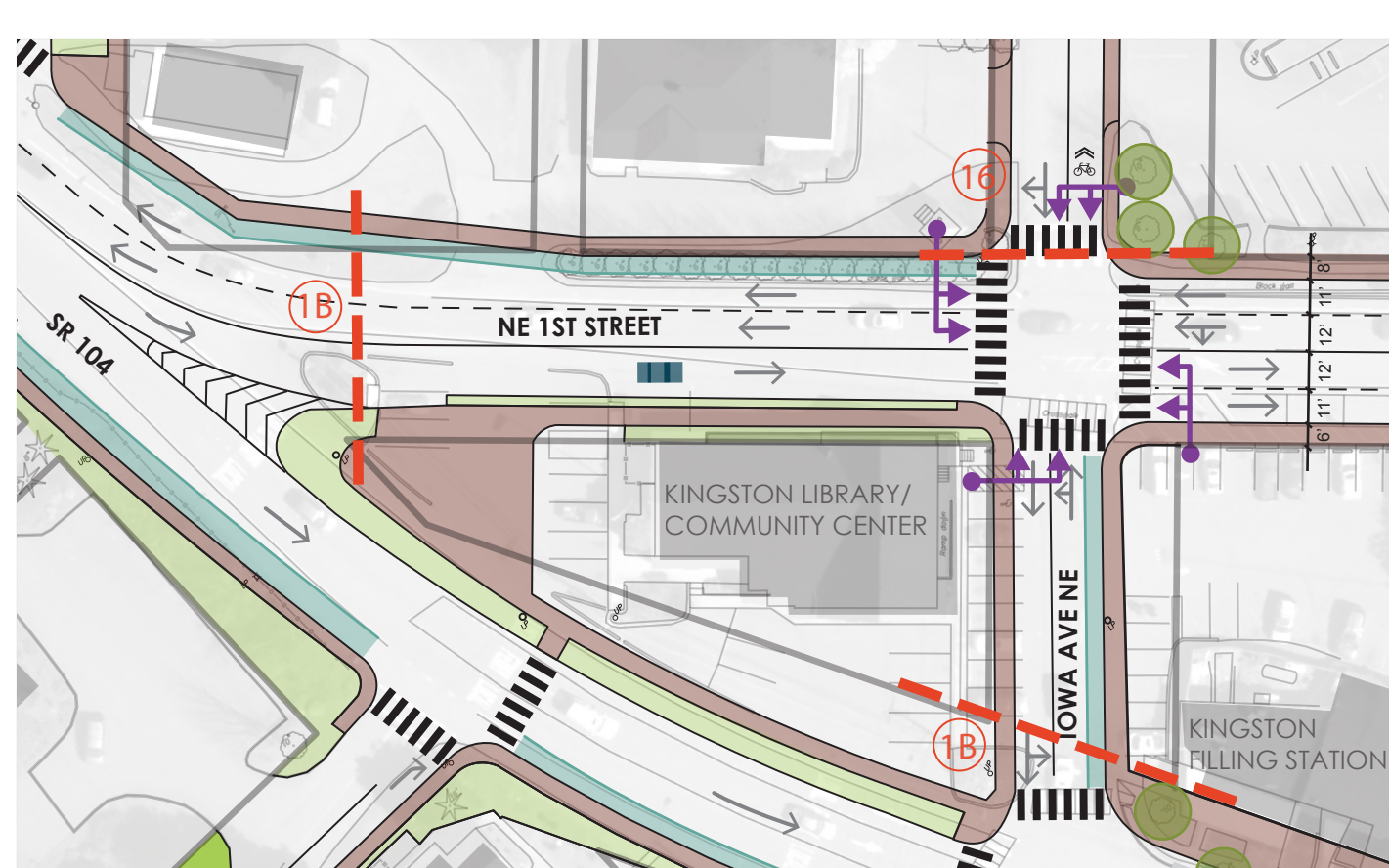
Relocation of ferry ingress to NE 1st Street has been a topic of discussion in the community and at WSDOT/WSF for decades. Moving ferry traffic one block off the main business district street would have the benefit of greater use of Main Street by pedestrians and bicyclists, and reduce vehicular traffic significantly. Some argue this change may adversely impact businesses, while others embrace this as a way to increase people-powered access to businesses.

### DESCRIPTION / FEATURES:

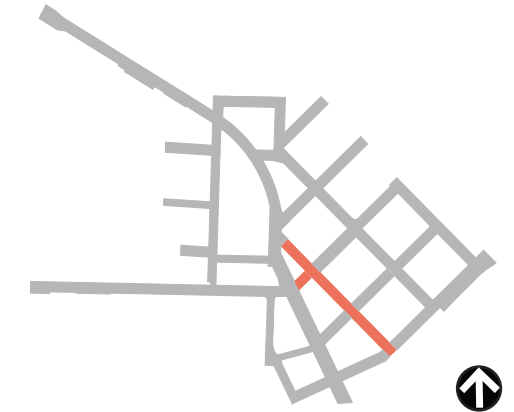
NE 1st Street is widened to 4 lanes, 2 lanes each direction serving ferry ingress and egress. NW (out) bound traffic allows for left turn on Iowa Avenue. SW (in) bound transitions from 1 lane to 2 lanes. Signals and crossing improvements are proposed at Iowa Avenue and Washington Boulevard. Sidewalks are added both sides, improving pedestrian access along this corridor where many walkers cross to WSDOT parking lot and to the residential area further north. NW (out) bound bicycle lanes from Main Street are routed on Iowa Avenue, then left onto 1st street (see Project 1B).



above: sidewalks and bike lane on SR 2



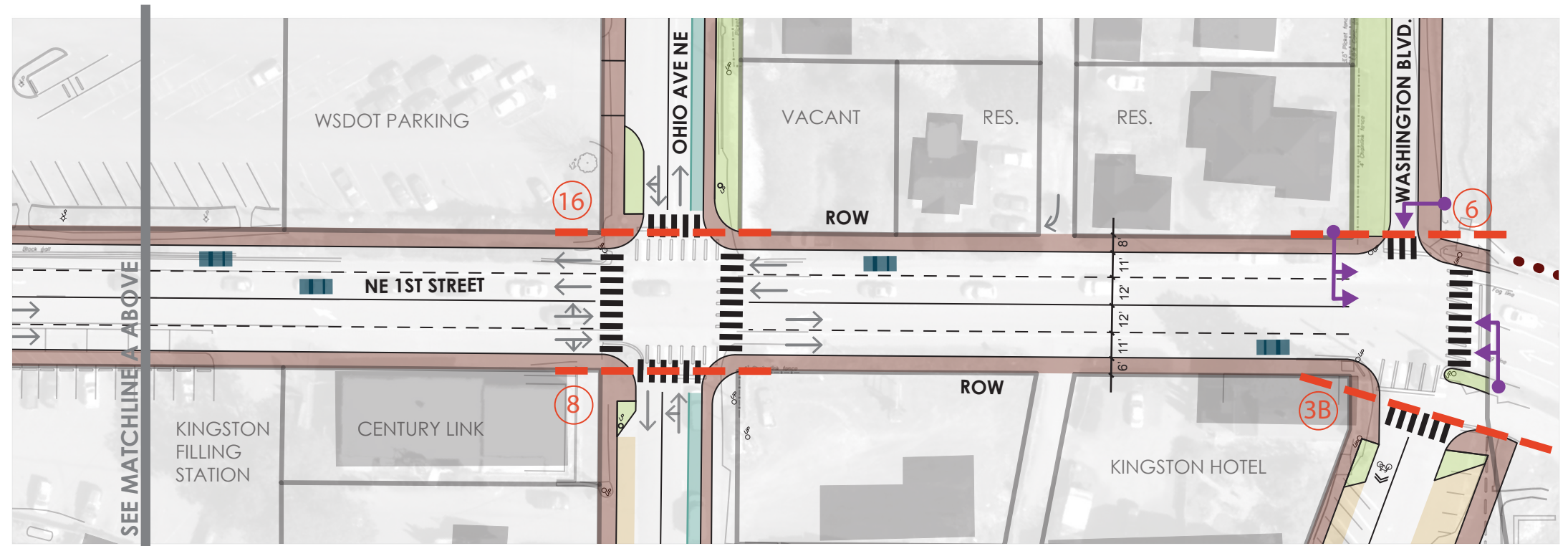
SEE MATCHLINE A BELOW



PROJECT KEY

COSTS: \$3,586,000

RANKING: HIGH



PLAN VIEW

1"=60'

- sharrow
- parking
- adjacent project number
- parcel line
- sidewalk/path
- traffic signal
- bioretention swale
- bike lane
- project limit
- vegetated buffer

